



US 212 Update

November 13, 2020

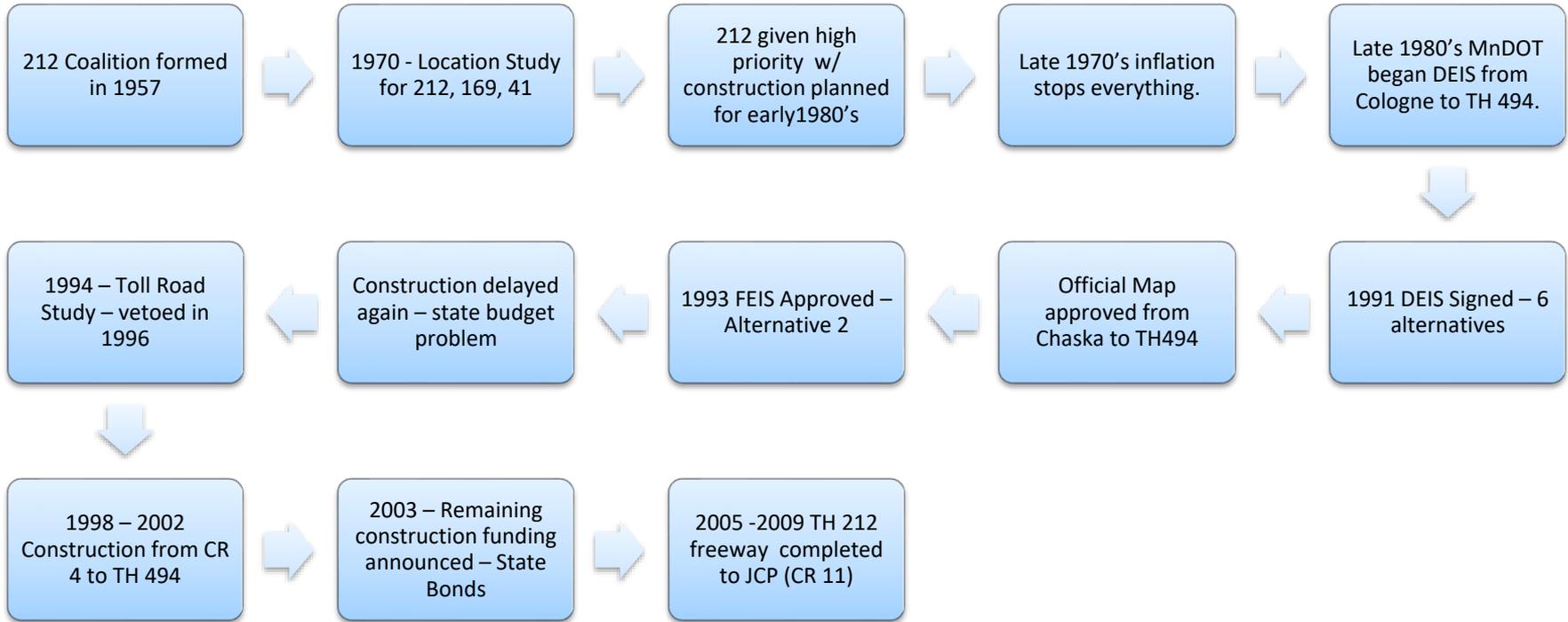


US 212 Update

- History
- Project Phases
- Background Information
- Phase 1 Update
- Phase 2 Update



History



US 212 Projects in Carver County



- 1 US 212 2-Lane to 4-Lane Conversion. **Completed 2009**
- 2 US 212/County Hwy 44 Interchange **Completed 2019**
- 3 US 212/County Hwy 53 Reduced Conflict Intersection **Completed 2012**
- 4 US 212/County Highway 36 to 41 Reduced Conflict Intersection **Completed 2019**
- 5 TH 5/TH 25/County Hwy 33 Intersection Improvements **Construction 2020**
- 6 US 212 Pavement Rehab and Intersection Improvements **Construction 2020**
- 7 US 212 Pedestrian Underpass **Construction 2020**



US 212 2-lane Gap Projects



- **Two phases**
 - Phase 1 (Dahlgren Township) – Carver to Cologne
 - Phase 2 (Benton Township) – Cologne to Norwood Young America



Background Information

- Serving **22,000 square miles** of rural Minnesota and South Dakota
- **Primary connector** for 65 major freight generators serving travelers from 74 of 87 counties
- **Only** 2-lane high priority interregional corridor in metro area
- Originally **constructed in 1930**
- **1,900 trucks** per day
- 305 crashes in 10 years; **ten fatal** and three serious injury crashes



Challenges & Goals

PROJECT CHALLENGES

- 1 ELIMINATE** the freight bottleneck
- 2 IMPROVE** roadway safety
- 3 EXPAND** access to employment
- 4 Ensure** state of good repair

The project addresses multiple transportation challenges including mobility and safety and reducing gaps in the transportation system to enhance connections between the greater western Minnesota region, South Dakota, and the Twin Cities.

Freight Bottleneck

17% increase in operational costs
Negatively affects 65 freight generators

The project will address critical capacity issues by creating a continuous, four-lane expressway from Glencoe to the Twin Cities.

Fatalities

5 fatalities in past 10 years

It will reduce fatalities and serious injuries in the corridor by eliminating two-lane rural highway gaps.

Employment Barriers

72% of residents travel outside Carver County for work

The project will expand highway capacity to strengthen US 212 as a major connection, linking rural communities to the Twin Cities economic hub.

Pavement Deterioration

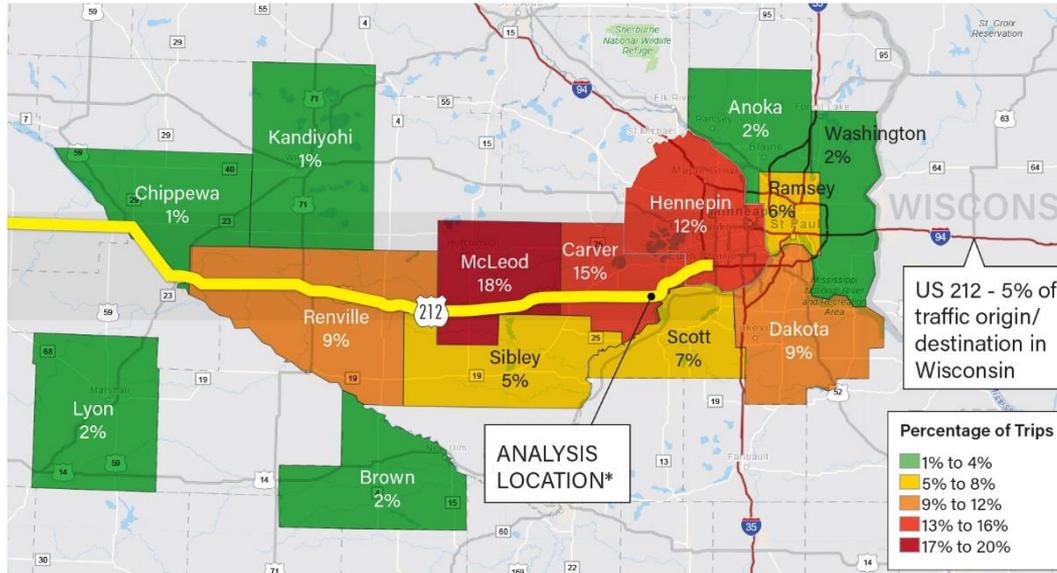
Pavement quality projected to deteriorate to "poor" by 2025

It will replace aging infrastructure that has not been expanded or reconstructed since it was originally built in 1930.



Eliminate Freight Bottleneck

CRITICAL FREIGHT CORRIDOR



US 212 freight trips by county.

Many production inputs at our 1,500-person Hutchinson facility come via the Highway 212 corridor. Any delay in receiving these inputs hurts our bottom line.

- 3M

Expanding Highway 212 to four lanes will save us time and money, but the safety benefits of the expansion are the most valuable to us.

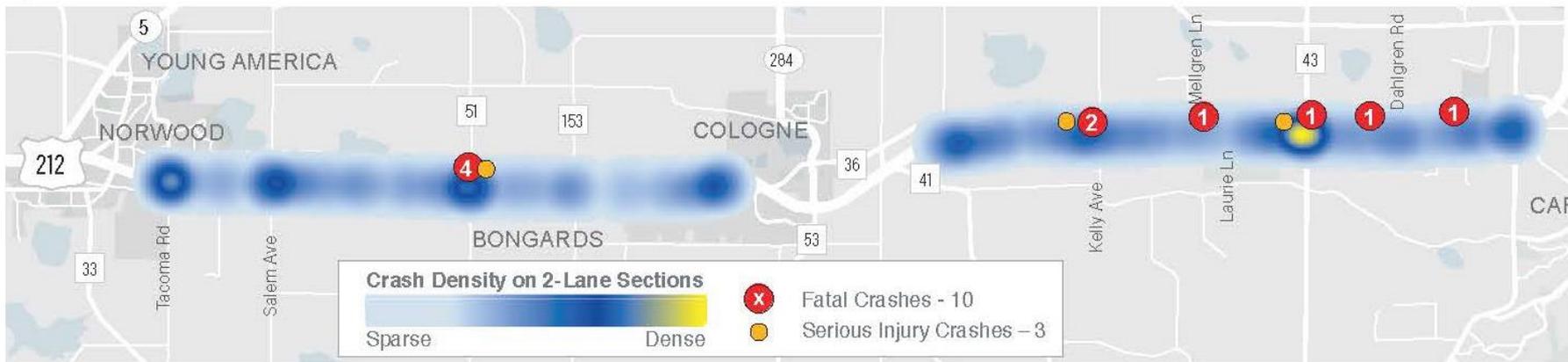
- Michael Foods Inc.



Improve Safety

Crashes

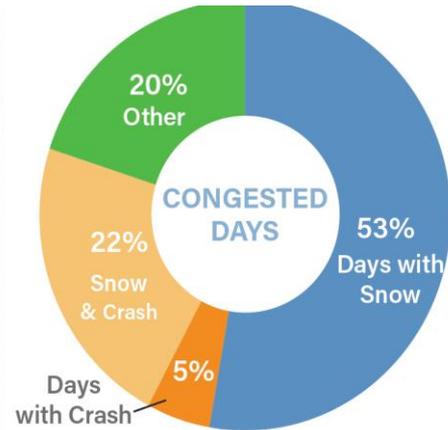
Within the project area, there have been **ten reported traffic fatalities** and three severe crashes involving life-threatening injuries (2009-2019).



Improve Travel Reliability

Congestion

75% of congested days on the corridor were due to a snow event or a combined snow and crash event.



Travel Time Delay & Reliability

The existing traffic volumes currently exceed the capacity of a two-lane, undivided highway.



Proposed Improvements

4-lane Expressway with Reduced Conflict Intersections

- Phase 1 \$50 million
- Phase 2 \$61 million (\$66 M with CR51 Interchange)

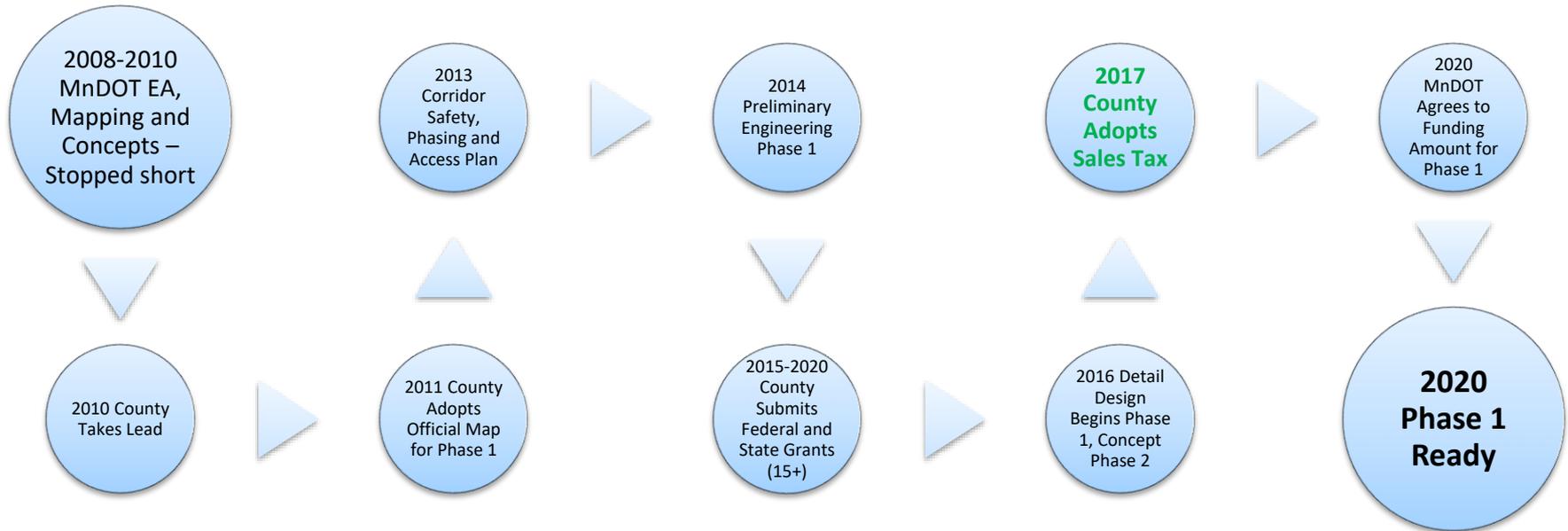


 Grade Separated Overpass (Option)

 Reduced Conflict Intersections (RCI)



Project Development



Federal Appropriations Requests



HPP	<ul style="list-style-type: none">• 2003: \$0.8M• 2004: \$1.45M• 2008: \$0.66M
HUD	<ul style="list-style-type: none">• 2010: \$2.4M
TCSP	<ul style="list-style-type: none">• 2011 :\$2M
AWARDS	<ul style="list-style-type: none">• \$2.9M



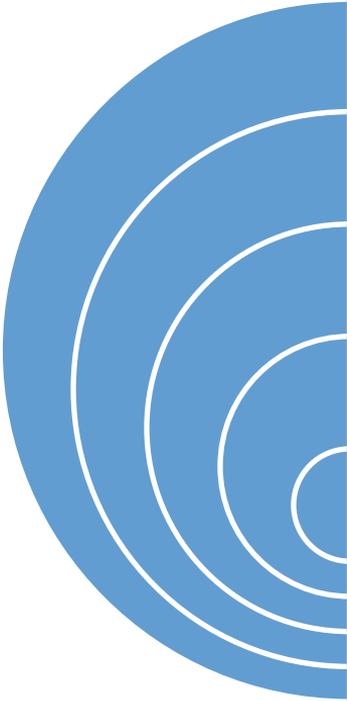
US DOT Funding Requests



BUILD	<ul style="list-style-type: none">•2020 : \$7.2M Phase 1•2019: \$25 M Phase 1&2•2018: \$12 M Phase 1, \$25 M Phase 2
INFRA	<ul style="list-style-type: none">•2020 :\$50M Phase 1&2•2019: \$42M Phase 1& 2•2017: \$21 M Phase 1
FASTLANE	<ul style="list-style-type: none">•2017: \$23.4 M Phase 1
TIGER	<ul style="list-style-type: none">•2010: \$0.8 M Planning
USDOT AWARDS	<ul style="list-style-type: none">• \$0



Mn Funding Requests



CoC	<ul style="list-style-type: none">• 2015• 2018
MHFP	<ul style="list-style-type: none">• 2017: \$15M Phase 1• 2020: \$10 M Phase 2
Regional Solicitation	<ul style="list-style-type: none">• 2018: \$7M Phase 1• 2020: \$10 M Phase 2, \$3.5M Phase 2
State Bonding	<ul style="list-style-type: none">• 2020: \$38M Phase 2
Mn AWARDS	<ul style="list-style-type: none">• \$22 Million



Phase 1 Details

- Update the **4.5 miles** of obsolete two-lane to **four-lane expressway**
- Construct **Reduced Conflict Intersections (RCIs)**
- **Agricultural Equipment Accommodations**
- **10' wide** shoulders
- **Turn lanes** at all public road intersections
- **New bridges** over Carver Creek
- **Several access modifications**

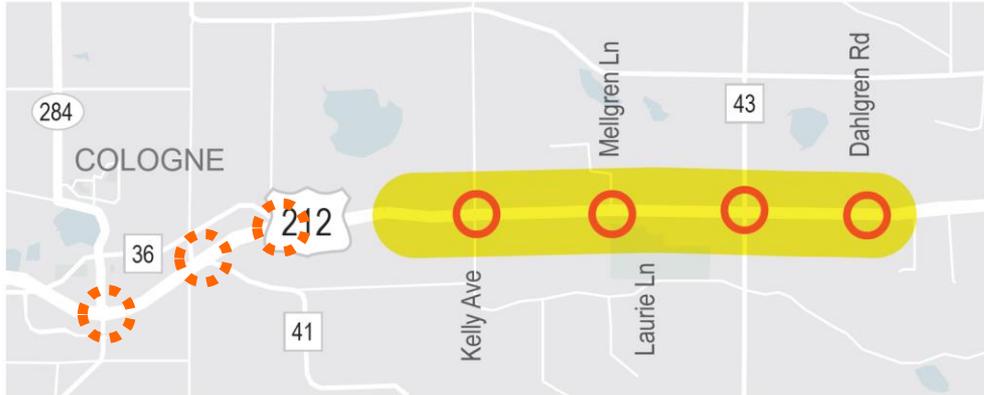


5 Miles of 2-lane to 4-lane expansion



Phase 1 Details

Reduced Conflict Intersection (RCI) Safety Improvements



 Reduced Conflict Intersections (RCI)

RCI Illustration



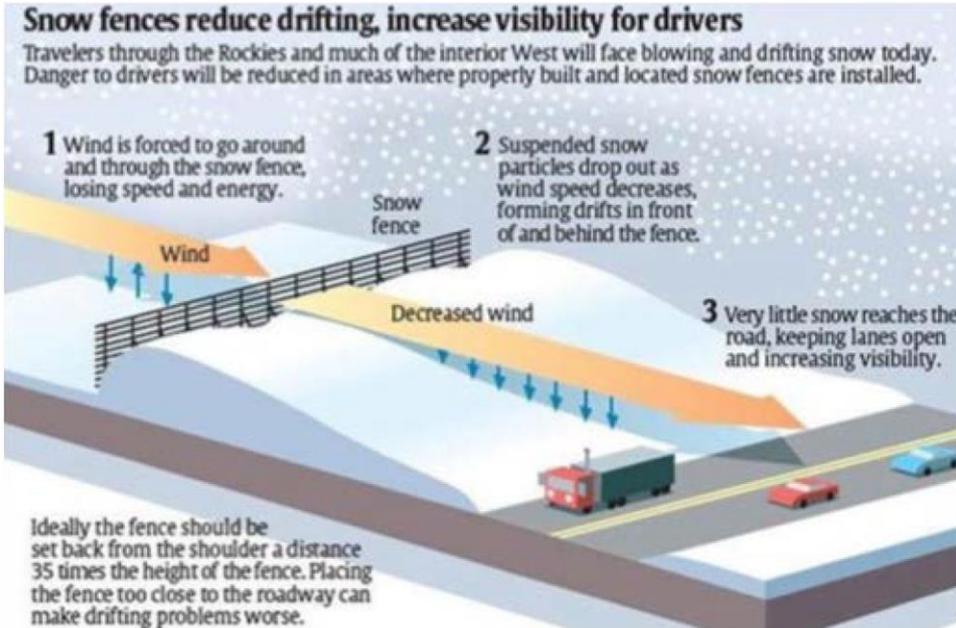
RCIs result in
70% reduction in fatalities
42% reduction in injury crashes

 **284/212 results: 100% reduction in fatal and injury crashes**



Phase 1 Details

Snow Fences



Phase 1 Details

- Key Work Items
 - **900,000+** Cubic Yards (CY) of Excavation
 - 150,000-200,000 CY of Excess Excavation
 - 96,000 tons of Bituminous Pavement
 - 80,000+ CY of Aggregate Base
 - Mumble Strips
 - Stormwater Treatment Meets WMO Requirements



Phase 1 Staging

- **Three Major Phases**
- **No Planned Closures of Hwy 212**
- **One Month Closure of Kelly Avenue**
- **One Month Closure of Hwy 43 South**
- **Three Month Closure of Hwy 43 North**
- **Coordination with Other Construction Projects**
 - **Hwy 212 from Hwy 43 to Hwy 11 is a portion of the detour for Hwy 11 in the Summer 2021**
 - **Coordination has occurred with local aggregate suppliers in the area**



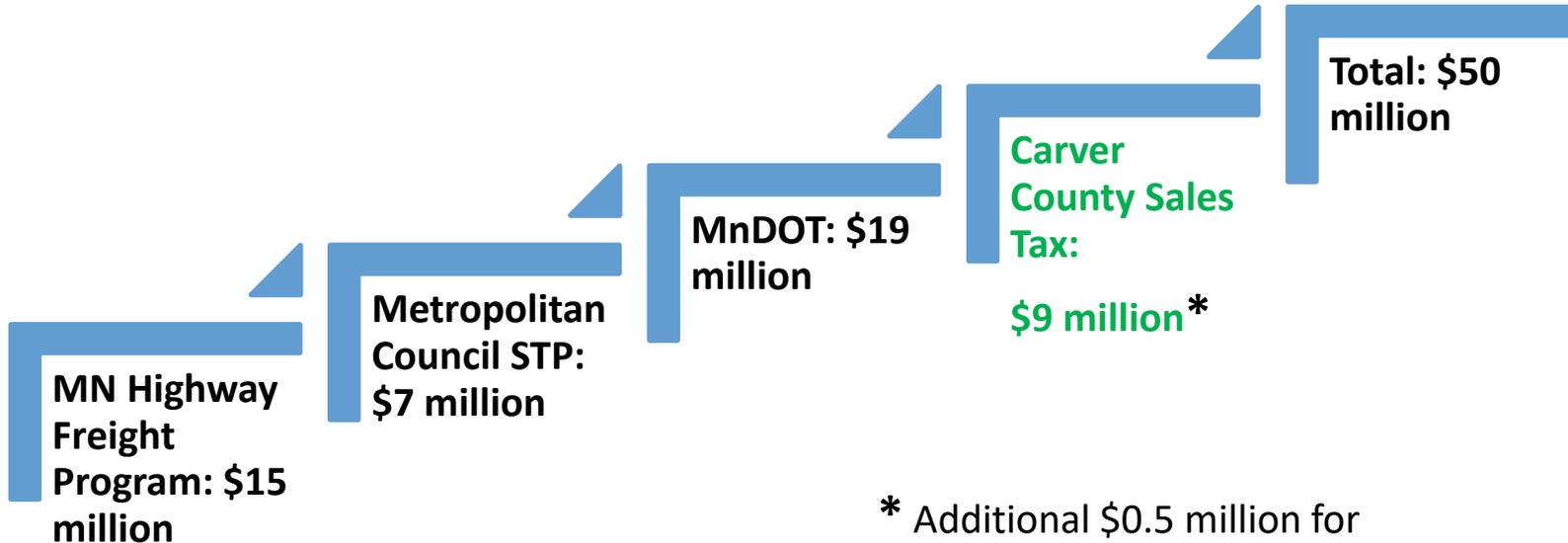
Phase 1 Costs

TOTAL
\$50
million

- Design = \$1.4 million
- Right of Way = \$5 million
- Construction = \$39.6 million
- Construction Contingency = \$2.0 million
- Construction Engineering = \$2.0 million



Phase 1 Funding



* Additional \$0.5 million for preliminary engineering. \$9.5 million. (\$1.5 million above current budget)



Phase 1 Schedule



*Hwy 11 Project Begins April 2021 and Ends November 2021.
Hwy 11 detour is coordinated with Hwy 212 Project.



Phase 2: Next Steps

- Environmental
- Engineering
- Right of way mapping
- Continue to Apply for Federal and State Grants
 - BUILD, INFRA
 - State Bonding
 - Build America Bureau Program, TIFIA



Phase 2 Costs

Total Project Cost = \$60.8 million*

- County Sales Tax: \$12 million
- MnDOT: \$4.7 million
- Pending Metropolitan Council Regional Solicitation: \$3.5 million
- **GAP \$40.6 million****

Potential funding sources

- MN Highway Freight Program: \$10 million
- Bonding, Corridors of Commerce, MnDOT
- Future BUILD or INFRA

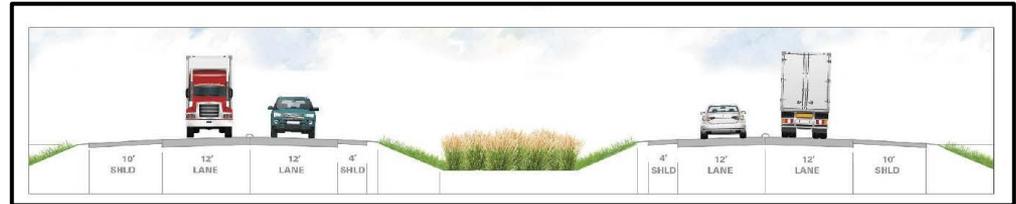
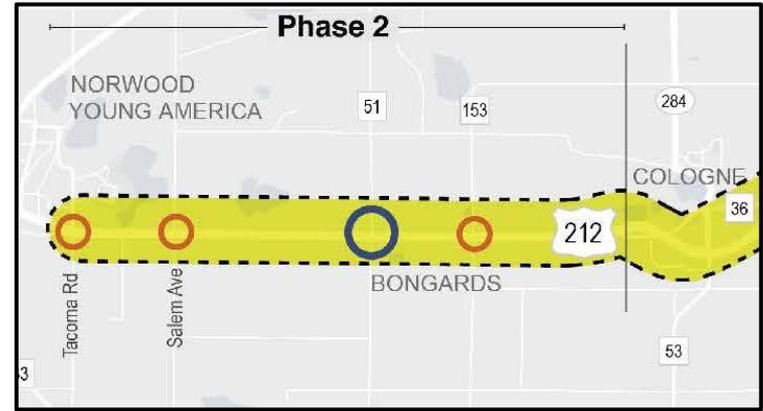


*\$66.3 million with Interchange at CR 51

**46.1 million with Interchange at CR 51

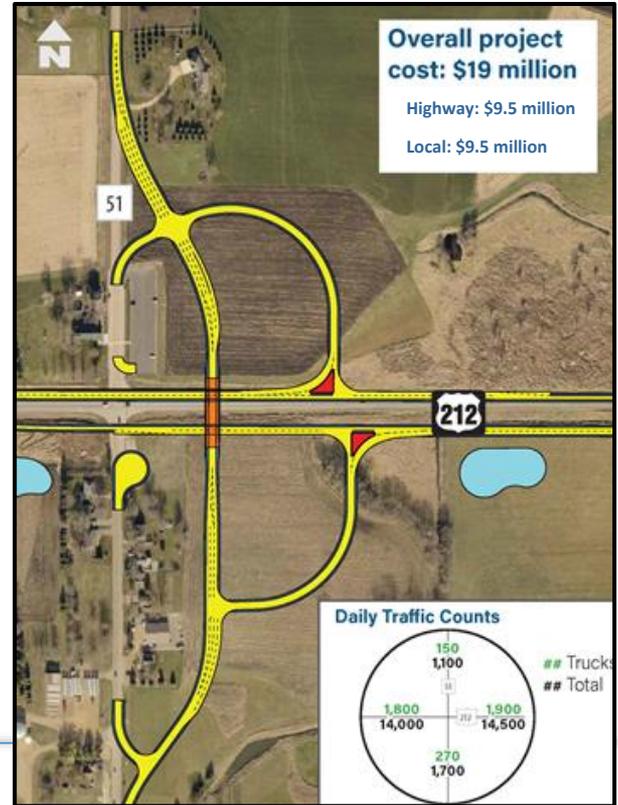
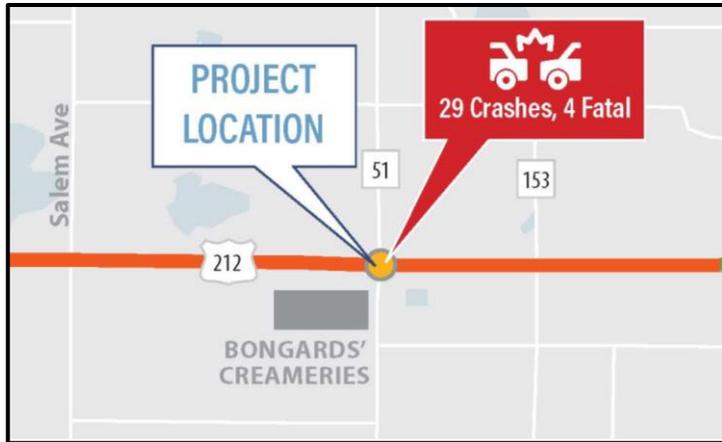
Phase 2 Bonding Request

- Request for Phase 2
- Bonding Request = \$38 million
 - MnDOT: \$25 m in TH Bonds for project development on locally led TH projects.
- Total Project Cost = \$66 million



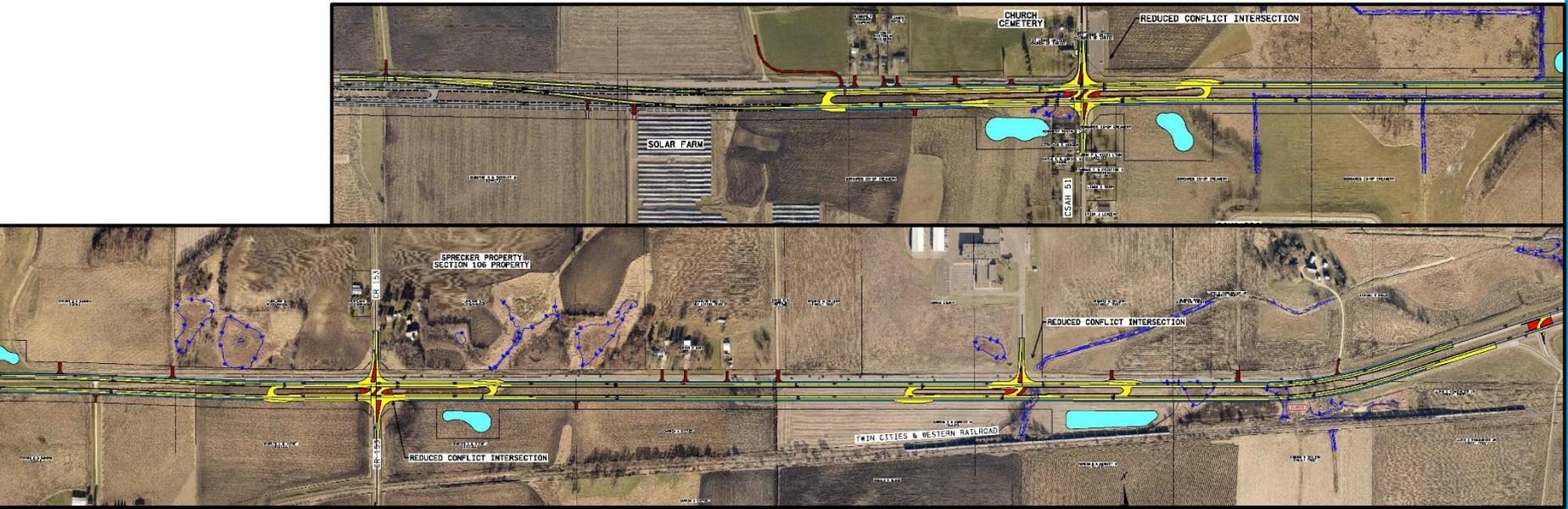
Phase 2 LRIP Request: US 212/CSAH 51 Interchange

- Funding Request = \$9.5 million
- Total Project Cost = \$19 million



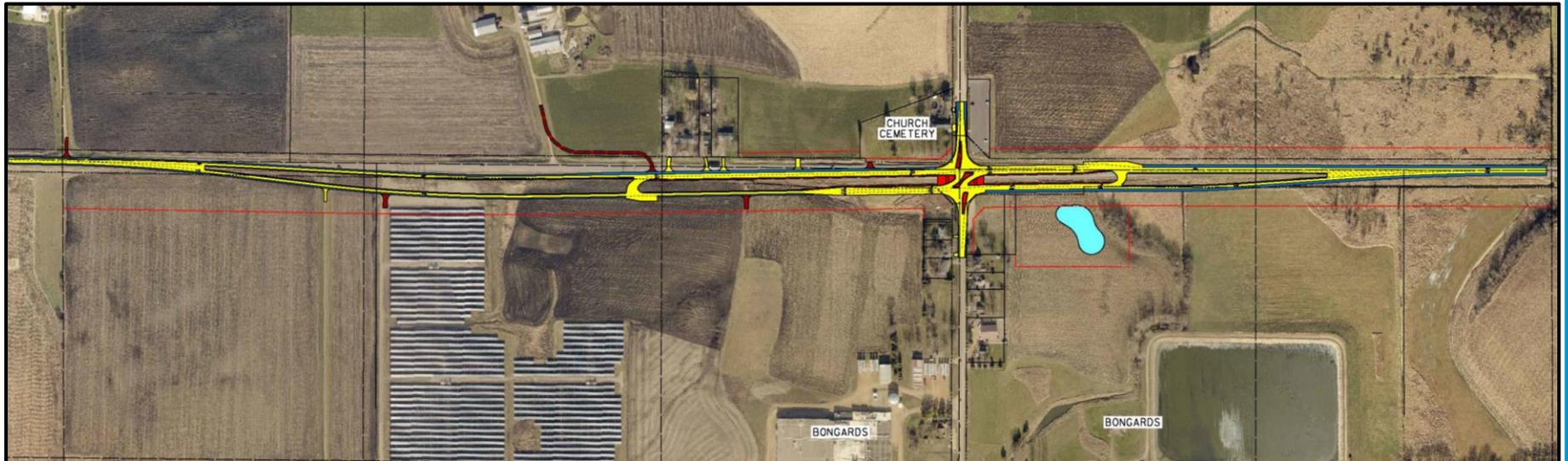
Phase 2 Regional Solicitation

- **Expansion** - US 212 Freight Mobility & Safety Project from **CSAH 51 to CSAH 36**
 - Expansion to 4-lanes with RCI at CSAH 51
 - Funding Request = \$10 million
 - Total Construction Cost = \$28.1 million
 - Awards announced December 2020



Phase 2 Regional Solicitation

- **Spot Mobility** - US 212 & CSAH 51 Intersection Safety Project
 - 4-lane divided RCI at intersection
 - Funding Request = \$3.5 million
 - Total Construction Cost = \$8.9 million
 - Awards announced December 2020



Phase 2 Schedule

Preliminary Engineering,
Environmental, Official Mapping 2021

Detailed Design 2022

Right of Way Acquisition 2023

Construction 2024



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