

US Highway 212 Rural Freight Mobility and Safety Projects



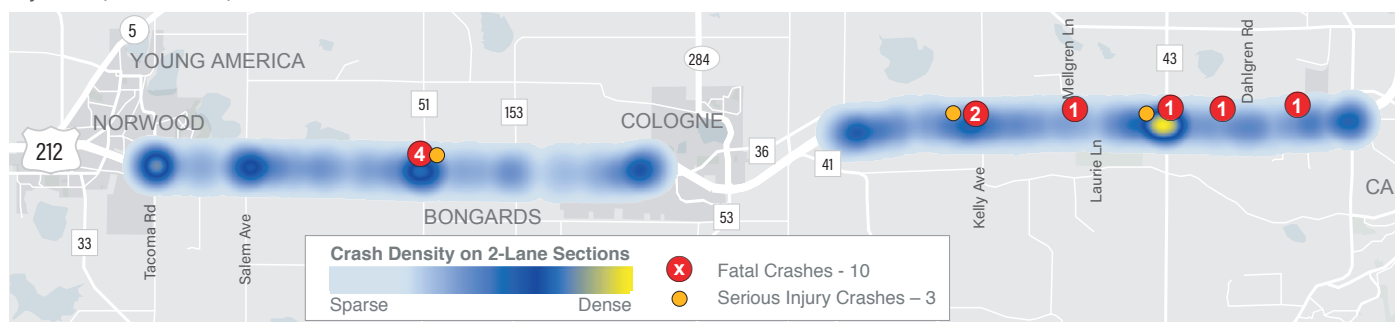
GOAL: Address critical capacity issues and eliminate 2 five-mile bottlenecks to improve regional and national travel time reliability and safety for residents, employees and others traveling through the corridor.

US HIGHWAY 212

- **Phase 1 includes five miles of 4-lane expansion** between Chaska and Cologne
- **Phase 2 includes five miles of 4-lane expansion** between Cologne and Norwood Young America
- **Serving 22,000 square miles** of rural MN and SD – Primary connector for 65 major freight generators
Serving travelers from 74 of 87 counties
- **Originally constructed** in 1930
- **Carries more trucks daily** (1,900) than the total traffic volume (both cars and trucks) on 40 percent of Minnesota highways.
- **305 crashes in 10 years;** ten fatal, 3 serious injury.
- **Only high priority interregional corridor in the metro area that still has two-lane segments.**
- **History of unsuccessful federal funding requests:**
FASTLANE 2017
INFRA 2017, 2019, 2020,
BUILD 2018, 2019, 2020,
TIGER 2010 (Planning)

CRASHES

Within the project area, there have been **Ten** reported traffic fatalities and **three** severe crashes involving life-threatening injuries (2009-2019).



PROJECT CHALLENGES

- 1 ELIMINATE** the freight bottleneck
- 2 IMPROVE** roadway safety
- 3 EXPAND** access to employment
- 4 Ensure** state of good repair

The project addresses multiple transportation challenges including mobility and safety and reducing gaps in the transportation system to enhance connections between the greater western Minnesota region, South Dakota, and the Twin Cities.

Freight Bottleneck

17% increase in operational costs
Negatively affects 65 freight generators

The project will address critical capacity issues by creating a continuous, four-lane expressway from Glencoe to the Twin Cities.

Fatalities

10 fatalities in past 10 years

It will reduce fatalities and serious injuries in the corridor by eliminating two-lane rural highway gaps.

Employment Barriers

72% of residents travel outside Carver County for work

The project will expand highway capacity to strengthen US 212 as a major connection, linking rural communities to the Twin Cities economic hub.

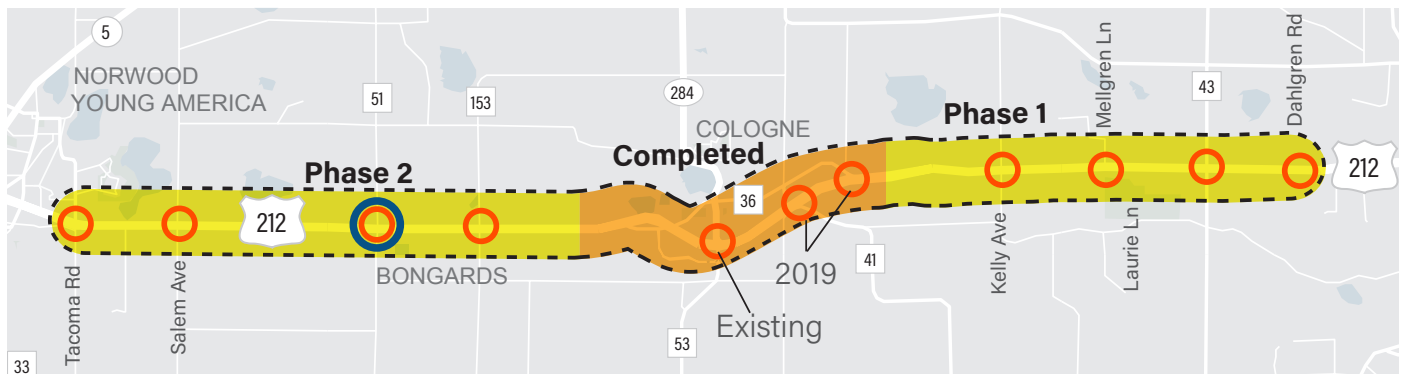
Pavement Deterioration

Pavement quality projected to deteriorate to "poor" by 2025

It will replace aging infrastructure that has not been expanded or reconstructed since it was originally built in 1930.

PROJECT IMPROVEMENTS

The Project will update the obsolete two-lane roadway to a four-lane expressway. It will address critical safety issues by reconstructing key intersections as Reduced Conflict Intersections (RCIs). Other improvements include the addition of full width shoulders, turn lanes, and replacement of bridges. If funding is available the CR 51 intersection will be replaced with a grade-separated interchange.

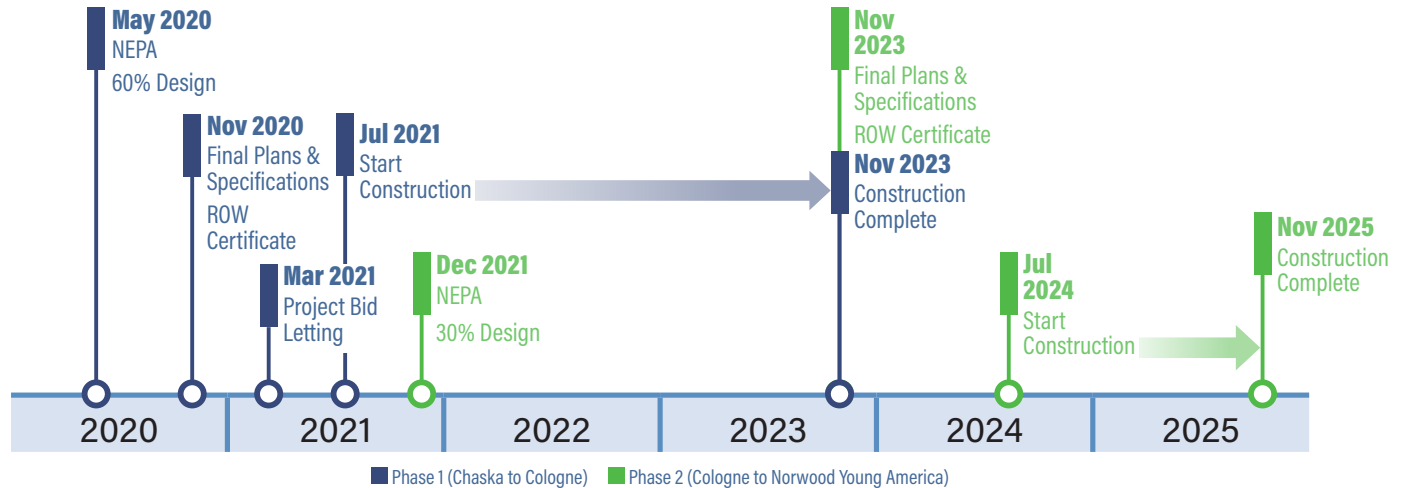


Grade Separated Overpass (Option)

Reduced Conflict Intersections (RCI)

Completed RCIs in the corridor have resulted in
50% reduction in injury crashes
100% reduction in fatal and serious injury right-angle crashes

PROJECT SCHEDULE



PROJECT COSTS/FUNDING

Phase 1 Costs

Federal Secured (Minnesota Highway Freight Program)	\$15,000,000
Federal Secured (Regional Solicitation)	\$7,000,000
County Secured (Local Option Sales Tax)	\$9,000,000
State Secured (SRC funds)	\$19,000,000
Total Phase 1 Cost	\$50,000,000

Phase 2 Costs

County (Local Option Sales Tax)	\$12,000,000
Federal (Pending Regional Solicitation)	\$3,500,000
State (SRC funds)	\$4,700,000
Gap	\$40,600,000
Total Phase 2 Cost	*\$60,800,000
Potential gap funding sources	
Future Federal Request (MHFP)	\$10,000,000
State (Bonding, Corridors of Commerce or MnDOT)	\$5,500,000 - \$10,500,000
Future Federal Request (BUILD/INFRA)	\$25,000,000

* Assumes RCI at CR 51. Interchange adds \$5,500,000

NEXT STEPS

- County and MnDOT moves ahead with Phase 2 project development and ROW mapping in 2021.
- County applies for upcoming grants: 2021 BUILD, 2021 INFRA, State legislative request, 2022 Met Council Regional Solicitation



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For more information:

[Carver County US 212 project](#)

#Make212Safe



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